

As of January 1,2010

New York Dwarf Car Association shall hereafter be referred to as NYDCA throughout the rules and descriptions. While Chapman's Can Am Motorsports Park Runs Mod Lites, the rules below are the exact rules that we will use. The rules and/or regulations set forth herein are designed to provide for the orderly conduct of racing events and to establish minimum acceptable requirements for such events. These rules shall govern the condition of all events, and by participating in these events, all participants are deemed to have complied with these rules. No expressed or implied warranty of safety shall result from publications of or compliance with these rules and/or regulations. They are intended as a guide for the conduct of the sport and are in no way a guarantee against injury or death to a participant, spectator or official. *The race director shall be empowered to permit reasonable and appropriate deviation from any of the specifications herein or impose any further restrictions that in his opinion do not alter the minimum acceptable requirements.* No expressed or implied warrant}' of safety shall result from such alteration of specifications. *Any interpretation or deviation of these rules is left to the discretion of the officials. Their decision is final*

## **GENERAL RULES**

1. All NYDCA vehicles are subject to inspection at any time.
2. Approval of a NYDCA vehicle by the inspector shall mean only that the vehicle is approved for participation in a competitive event and shall not be construed in any way to mean that the inspected NYDCA vehicle is guaranteed mechanically sound. Be it further declared that the inspector shall not be liable, nor shall the sanctioning body, for any mechanical failure not for any losses, injuries or death resulting from same.
3. NYDCA race rules shall apply to all NYDCA sanctioned events. Driver must hold a current NYDCA competition membership in order to receive points and payouts.

## **SPECIFICATIONS**

### **1. BODY STYLES**

Any car specifications not covered in these body specs are subject to NYDCA interpretation. Any significant performance advantage will be addressed on an individual basis. NYDCA reserves the right to determine what constitutes an unfair advantage.

- (a) 1928-1948 American-made production vehicles only, 5/8 scale, two-door sedans, coupes, or pickup trucks. These are to be known as Classic Modified body styles.
- b) Modified appearing body style such as IMCA or DIRT modified. These are to be known as Modified Lite body styles.
- c) All chassis specifications are the same for both body styles.

### **2. BODY REQUIREMENTS AND SPECIFICATIONS**

#### **2a. CLASSIC MODIFIEDS**

- (a) Body must be 5/8 scale, stock appearing. Maximum outside tire width 60", Cars must pass freely through a hoop 60 1/2" wide by 50" high, 73" wheelbase plus or minus 1".
1. Must have grill shell or simulated open radiator matching original body style, any material.
2. Doors, windows, and cowlings must retain stock appearance. Hood louvers allowed for cooling, but must maintain stock contours.
3. Engine compartment must conform to scale and match contour of body. Hole may be cut for breather clearance only. Ram air induction system prohibited

4. Must have closed trunk lid or fully enclosed trunk access panel conforming to original bodylines. Louvers allowed no holes.
5. No fenders.
6. No wings or spoilers.
7. Ram air induction system prohibited.

b) Body will be metal construction, 26-gauge steel or .040" aluminum minimum. Fiberglass or plastic grill shells allowed. All exposed edges must be folded or trimmed over. Legends cars may be allowed under local rules. No lips or spoilers of any kind.

c) NO open top cars (roadsters, convertibles). No panel trucks, station wagons, or specialty cars.

d) Classic Modified Overall Dimensions

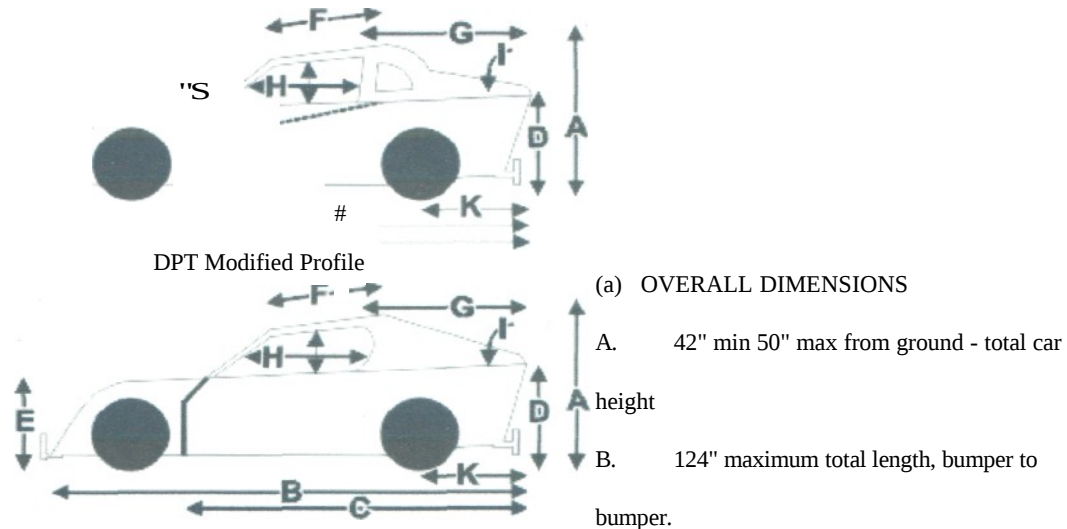
1. NO car over 50" tall, top to ground. Body height maximum 44", bottom edge to top (including frame rail).
2. Maximum body width: 38" Maximum frame width 34"; min 30"
3. Body contour must follow frame and roll cage. Roof hatch entrance allowed.
4. Frame ride height must not let car bottom out on track.
5. Drivers door must be operational and equipped with secure latch.
6. Fasteners on hood, trunk, or panels must be positive, locking type.
7. 124" maximum total length, bumper to bumper.
8. Kick outs or side nerf bars run the full length of the car between tires. They must be 1/4" maximum 0.125 wall thickness. They cannot stick out beyond 1" from the side of the tire. Mounting points to the frame must be made in at least 2 points, however, if two points are used, there must be a hoop formed in the bar, facing downwards between the connection points.

### MODIFIED LITE

The intent of these preceding specifications is to follow the bodyline and design of an exciting IMCA or DIRT modified racecar.

MCA Modified Profile

#### 2b. MODIFIED LITE



C. SIDES AND REAR QUARTER PANEL

1. 84" max - Door and rear quarter length.

D. 30" min 38" max - side height of rear quarter panel from the ground. 30" max height of quarter panel.

E. HOOD

1. 20" min 27" max - hood height excluding breather protectors (bottom of frame rail to top of hood). Ram air induction system prohibited. Rear of hood becomes part of windshield area rules. No part of nose section (in front of spindle centerline) may be wider than a maximum of 24". Hood may taper out to door panels at the rear.

F. ROOF PANEL

1. 40" min 48" max-length of roof panel. 36" min 42" max-width of roof.

No part of roof panel may have more than 14° of angle rake. No deflector of any type on upper sides or rear of roof panel. Roof entrance hatch optional.

G. REAR UPPER QUARTER PANEL

1. 20" min 38" max rear upper side panel may contain Plexiglas. Side panels maybe no higher than 3" at the rear of the straight Line of quarter panel top edge. Side panel may be no higher than an imaginary line from the rear edge of roof panel to 3" above the top edge of the rear quarter panel.

H. SIDE WINDOW

1. 18" min side opening length. 10" minimum side opening height front and rear.

I. INTERIOR

1. 46" min 50" max - width of rear shelf and body panels. Rear shelf may have no more than a 5° max rake and may not be concave or convex in design. No lips or spoilers of any kind. The rear frame area from the rear shelf downward a minimum of 16" must be completely enclosed the width of the frame. Louvers allowed, no holes.

J. ADDITIONAL INTERIOR SPECIFICATIONS

1. 36" min 56" max - length of right side pod area with max 30°. Left side pod may be altered for driver access.
2. Rear quarter distance from center of rear 20" min. 30" max.
3. Cockpit cover with a max of 1/2" lip will be permitted providing the on/off cutoff switch is reachable from outside the car by safety personnel.
4. Deck lid height from ground 30" minimum, 36" maximum with NO rear lip extending upward.
5. Side pod may not extend rearward past the centerline of rear axle.
6. Body must be centered on chassis.
7. If windshield is used, it must be flat.
8. All body dimensions will be teched + or - Vi" unless otherwise specified.
9. Rear Spoiler may be used and will be a maximum of 4 inches attached to the rear decklid only.

**3. WEIGHT**

- (a) All cars following (a) "NYDCA 600cc Engine" motor program must weigh 1100 lbs, with driver after race.
- (b) All cars following (b) "NYDCA Pure Stock" engine program (1000cc) must weigh a minimum of 1260 lbs, with driver after race.
- (c) All cars following (c) "NYDCA Spec Engine" motor program must weigh 1300 lbs, with driver after race.
- (d) All cars following (d) "NYDCA Open Engine" motor program (1074-1200cc) must weigh 1325 lbs. with driver after race.

- e) No weight outside of car body, i.e. on nerf bars
- f) Hood will be clearly marked with cc displacement and minimum weight as outlined in Engine Specification, Paragraph 11.
- (g) This weight program will be monitored for parity and competitiveness and may be changed at the discretion of NYDCA.

#### **4. BUMPER DIMENSIONS**

- a) Front maximum width 30", Bumper height 6" to 8" with 2-bar loop. Ground to top of bumper: 12" minimum, 16" maximum. Bumper may not extend more than 12" forward of front tires.
- b) Rear maximum width 56" with 3 vertical bars. Bumper height 6" to 8" with 2-bar configuration. Ground to top of bumper: 12" minimum, 18" maximum. May not extend more than 8" behind rear body panels.
- c) Bumpers must be hollow with max wall thickness of .125

#### **5. FIREWALL**

- (a) Firewall must be metal and must completely separate engine compartment from cockpit. Engine protruding into cockpit must be enclosed.

#### **6. WINDSHIELD**

- (a) Windshield must be Lexan, wire mesh, or protection bars. Lexan highly recommended when running asphalt. No Glass. All other cockpit areas must remain open. Deflectors may be used in front of driver. May be no wider than roll bar area.

#### **7. NERF BARS**

- (a) Full length nerf bars between front and rear tires mandatory. Maximum outside diameter of 1.25" and .125 maximum thicknesses, must be hollow and may not extend out beyond tread width more than 1" at the rear only.

#### **8. SEAT**

- (a) Seat must be high-backed aluminum, racing type. All cars will have a permanent bar located behind top of seat at shoulder level. Bottom of seat will not be more than 63" back measured from upper ball joint. Seat may be tilted back for added driver head clearance, however, no portion of the seat may be back more than 70" as measured above. Seat must be secured at the bottom with four 7/16" bolts and nuts.

#### **9. NUMBERS AND GRAPHICS**

- a) Car number must be minimum 12" in height and 2" stroke. Must be displayed on each door and roof or trunk. Numbers in 4" size should be on front and rear of car to help with line-ups.
- b) You must indicate on the sides of your hood, the engine program in use. Examples are as follows:

A) Pure Stock GSXR1000 1260 lbs.

B) Spec Engine GSXR1000 1300 lbs.

C) Open Engine GSXR1070 1325 lbs.

## 10. WHEELS AND TIRES

- a) Steel wheels only, minimum wall thickness .090", 13" only, Wheel width max. 8".
- b) All classic modified body styles and Modified Lite body styles will be allowed to use Hoosier RC1, RC3, RC4, TUSA, or UDRA plated DOT tires. All tires for Classic modified body styles will have an unlimited durometer. All tires for Modified Lite style bodies must durometer 40 before any racing event
- c) Bead lock rims are optional, and can be used on any or on all four wheels.

## 11. ENGINE SPECIFICATIONS

### (a) "NYDCA Pure Stock Engine Motor Program" Specifications:

1000cc OR LESS WATER - COOLED ENGINES BETWEEN THE YEARS OF 2000 AND 2008 CAN BE USED IN THIS PROGRAM. MINIMUM WEIGHT REQUIREMENT FOR THIS PROGRAM WITH DRIVER WILL BE 1260 POUNDS

1. Engine must remain in its pure stock factory manufactured condition.
2. Bore and stroke must remain unaltered from a factory manufactured specification.
3. No altering of pistons, rods, head components, cams, or cam sprockets. No after market head gaskets will be allowed. Milling of head will not be allowed.
4. Only factory supplied ignition and ECU box will be allowed.
5. No fuel monitoring devices will be allowed.
6. Engines must remain with injectors or carburetors that were supplied with engine from the factory. You will not be allowed to interchange any component that was not factory supplied with your engine.
7. After market air boxes, or those manufactured or supplied by chassis builders will be allowed. After market air filters, such as K&N in the same configuration as the stock air filters, may be used.
8. Alternators must remain on the engine, and cannot be removed.
9. Engine parts from different year engines cannot be swapped between engines.
10. After market valves cannot be used.
11. Factory balancer and/or balancer shaft must remain in engine in unmodified form and in original placement.
12. Freshening of any pure stock engine must only involve the replacement of any worn or faulty parts, with factory manufactured parts intended for use on that particular engine.
13. One piece billet, or cut down oil pans may be used.
14. Engines manufactured below 1000 cc, may be used in the pure stock engine program, if the engine is factory supplied with carburetors. Engines below 1000 cc with factory supplied fuel injectors may not be used in the pure stock program. Aftermarket air boxes and air filters will be allowed on carbureted engines below 1000 cc.
13. The intent of the pure stock engine program is to use engines directly from a factory supplied street legal motorcycle, without any modifications whatsoever. If there is something that has not been addressed in the above specification, do not assume modifications can be made. You must request clarification from your governing body with any item that has not been directly addressed.

Allowable engines that may be used in this program will be:

Suzuki GSXR1000  
Honda CBR1000RR  
Yamaha YZFR1  
Kawasaki ZX10

### (b) "NYDCA Spec Engine Program" Specifications:

1000cc OR LESS - WATER-COOLED ENGINES PRODUCED BETWEEN THE YEARS OF 2000 AND 2008 CAN BE USED IN THIS PROGRAM. MINIMUM WEIGHT REQUIREMENT FOR THIS PROGRAM WITH DRIVER WILL BE 1300LBS.

1. Must remain stock bore and stroke, no altering rod or piston specifications.
2. All head components must remain stock, (except cam sprockets may be slotted).

- 3.Cams must be as per original equipment manufactured (OEM) and must meet specs as per manufacturer.  
Cam shafts must remain stock lift and duration and be in their original placement, (i.e. Intake on intake side, and exhaust on exhaust side).
- 4.OEM stock ignition system only. Power commanders, TFI or any fuel monitoring devices can be used, but may not include any additional boxes, modules, or devices to alter motor function. Bazzaz fuel performance devices will not be allowed.
- 5.Engine must retain its stock fuel induction system. May use after market air filter systems.
- 6.Alternator recommended but not mandatory.
- 7.Parts from different year engines cannot be swapped between engines.
- 8.Effective in 2012, no modifications to the bottom of the engine will be allowed. All bottom end components must remain in stock configuration. Lightening of crankshafts will not be permitted. 2012 Weight requirements will be confirmed at a later date.

**(c) "NYDCA Open Engine Program" Specifications:**

ENGINES ALLOWED BETWEEN THE YEARS OF 2000 AND 2008 CAN BE USED. MINIMUM WEIGHT REQUIREMENT FOR THIS PROGRAM WITH DRIVER WILL BE 1325 LBS.

- 1.Side draft motors 1250cc (+1%).
- 2.Down draft motors limited to 1200cc maximum with no variance.
- 3.ZX12-must remain stock bore, stroke, rod length, and be stock appearing. Stock OEM fuel injection only.  
All head components must be OEM stock with the exception of cam sprockets. Only OEM stock ignition with OEM ECU box can be used. Power Commanders, TFI, or other fuel monitoring devices, wired into the harness, with no connections, will be allowed. No fuel monitoring control devices allowed in the drivers reach or driver cockpit area. Bazzaz fuel performance devices will not be allowed.
- 4.Water-cooled engines HOOcc and below can run carburetors or fuel injection systems. Engines above 1 IOcc OEM must remain in their original stock configuration of carburetors or fuel injection.
- 5.Down draft engines must run gasoline only.
- 6.No engine can be modified to be less than original stock configuration OEM specifications.
- 7.Engines must be in production for a designated time period to determine its competitive relationship with our current motor program. Any major changes to the existing engine by the manufacturer must be reviewed by the NYDCA board before approval.
8. OEM electronic engine components (Power Commanders permitted), will be reviewed annually to ensure equality.
- 9.Carburetors and cams optional (except ZX12).
- 10.After market cylinder heads will not be allowed on any engine.
- 11.Porting, grinding, polishing, or changing stock of configuration of intake or exhaust ports allowed on oil-cooled motors. Carburetor / fuel injection boot must fasten directly to head in the stock intake port location.
- 12.No porting, grinding, polishing, or changing stock configuration of intake or exhaust ports allowed on water-cooled motors. Casting part lines must be visible upon inspection. Intake port area may be altered to match intake boot. Alterations not to exceed 3/8" into intake port. Carburetor / fuel injection boot must fasten directly to head in the stock intake port location.
- 13.Header must attach directly to head in stock exhaust port location.
- 14.Oiling system may not include a dry sump system.
15. No increasing or decreasing of motor stroke in any way.

**NOTES**

- 1.Weight allocations for all engine programs can be reviewed based on track performance and may be altered to suit the competitive nature of modified lite racing.
- 2.You must indicate on the sides of your hood, the engine program in use. Examples are as follows:

- A) Pure Stock GSXR1000 1260 Ibs.
- B) Spec Engine GSXR1000 1300 Ibs.
- C) Open Engine GSXR1070 1325 Ibs.

**(d) "General Motor" Specifications:**

1. Four cycle, four cylinders maximum only. Must have working transmission, clutch, and starter with all parts in place. Complete Clutch assembly must remain in engine. External reverse boxes allowed. No air controlled shifting devices.
2. No auxiliary starters.
3. Engines must be front mounted in engine compartment.
4. Engine setback: The "square of the engine" (rear fins of the jugs) may not extend rearward more than 17" from the center of front spindle. No part of the engine or transmission may extend rearward more than 21 inches from center of front spindle.
5. No after market fuel injection, and no turbos. No nitro, or nitrous oxide.
6. Engine must be cooled by original intent. May use extra fan or oil cooler.
7. Engine exhaust must include muffler and exit rearward. All muffler devices must include baffles.  
**MUFFLER MUST BE A MINIMUM OF 95 DECIBELS AT 25 FEET OR MEET LOCAL TRACK REGULATIONS, (IF MANDATORY).**

**(e) Engine Tech Procedures**

1. Cylinder cranking compression- minimum 100 p.s.i., Maximum 220 p.s.i. per cylinder no averaging allowed,, no variance, measured with a Snap-On compression gauge with adapters, after ten (10) revolutions of cranking, 15-45 minutes following feature event. Top three finishers of the feature event will be teched, or quantity as determined by NYDCA officials.
2. Carburetors or Fuel Injection and all four (4) spark plugs must be removed before checking compression.

**12. BATTERY, FUEL SYSTEM**

- a) Battery and fuel pump must be mounted outside driver compartment. Battery must be secure and held with non-rubber strap. Electric fuel pump allowed.
- b) Fuel cell mandatory, maximum 5 gallons, located in trunk.
- c) Gasoline or alcohol only, no fuel additives allowed. According to local preference.
- d) Reinforced or steel braided line must be used in all high-pressure areas.
- e) A direct manual cutoff switch wired to ground circuit will be installed on the right rear shelf or rear fire wall within 6" of the corner formed by the rear deck and rear fire wall. Switch will be located over the right shoulder of the driver. The emergency electrical cut off switch will be clearly located and marked.
- f) All terminals to electrical equipment in trunk area must have rubber coverings over all terminal connections.

**13. DRIVE TRAIN**

- a) Must use steel drive shaft from transmission to automotive rear end. Minimum 2.50" x .083" or 2" x .120".
- b) Quick change rear ends allowed with steel tubes only.
- c) Drive shaft tunnel must have minimum of 2 (3 recommended) 360-degree hoops between firewall and seat back. Must be fabricated of minimum .1875" X 1.5" steel bar. 1" X .095" steel tube highly recommended.
- d) Rear must be locked or have a solid spool.

#### 14. FRAME AND ROLL CAGE

- a) Main frame members will be minimum of 1" X 2" x .120" wall rectangular steel tubing only. Mainframe rail consist of the length between the front lower a-arm cross member and the rear main roll bar upright. No aluminum allowed in cage or frame.
- b) Main frames will have a minimum of 3 cross members between firewall and back of seat fabricated of same material as frame rails. Alternate material may be steel angle, min. .125" X 1.5" X 2". Cross-bracing in floor under driver strongly recommended. Rear frame rails at rear roll bar must be 34" max and 30" minimum outside dimension. Rear roll bar must meet same dimension as the frame it is mounted to. Frame may taper forward of driver seat area. Front engine area frame width must be 24: max and 20" min outside dimension. Left and right frame rails must be the same + or - 1/4".
- c) Roll cage must be DOM steel minimum 1.25" x .083" (.095 recommended) in all cars manufactured from 12/98 on. Clubs may grandfather older cars with 1" x .065" tubing. Cross-brace or diagonal strongly recommended in hoop over driver's head.
- d) All cars must have cage bars installed across door openings on both sides. Bars should be welded in place as part of roll cage. Minimum size 1" x .065". (1.25" x .095" recommended) cars with bars mounted on doors should have full doorframe and minimum of 2 horizontal cross bars. Minimum 1" x .083". Cars with bars mounted on doors must have 1/4" x 2" steel stops welded in on each side of opening, totaling a minimum of 4" on each end of door.
- e) It is required to have a steel bar with a minimum size of 1" X .065" from doorpost to doorpost, under the dash. Steel plate on driver's door bars recommended.
- f) It is recommended that, if using rivets, body panels attach to tabs welded onto cage instead of drilling multiple holes into cage bars.
- g) No down tube frame cars allowed. Any bars going from cowl area to snout must remain below the upper line of hood.
- (h) Classic pickups may have two (2) bars going from upper rear roll cage down to rear clip.

#### 15. SUSPENSION

- a) Front suspension must not extend rearward past the most forward panel of the firewall.
- b) Rear trailing arm/radius rod specifications:
  - 1. Arms may have a maximum length of 30".
  - 2. Torque-absorbing devices are permitted.
  - 3. Maximum 3 radius rods or 3 link suspension
- c) Wheel offsets are allowed.
- d) Hub offsets are NOT allowed. Hubs front and rear must be within 1/2" distance from mainframe rail on either side of car, measured from center of rotor to vertical plane of frame rail.
- e) Coil-over shocks are the only allowable means of supporting the chassis. One shock and spring per wheel only.
- f) Shocks must be of all-steel construction, non-adjustable type. May not be of a type that allows disassembly.
- g) No driver shall have the ability to adjust suspension from inside car.
- (h) Straight front axles are prohibited. Local clubs may grandfather existing cars for competition,
- (i) No leaf springs.

- (j) No birdcage type rear linkage.
- (k) Anti-roll (sway) bars permitted front or rear. Must be connected without adjustments to arm location.
- (l) No torsion bar suspension system of any type allowed.

#### **16. STEERING**

- a) All steering components must have safety fasteners such as cotter pins or self-locking nuts.
- b) Manufactured quick-release steering wheel hubs mandatory.
- c) Rack and pinion steering is mandatory.
- d) Collapsible steering shaft highly recommended.

#### **17. BRAKES**

- a) All four wheels must have working calipers and rotors.
- b) Rotors and calipers:
  - 1. Steel rotors mandatory, .250 minimum thickness.
  - 2. Steel OEM calipers or aluminum calipers.
  - 3. Cutting & scalloping of rotors will be allowed with no rotor surface being less than 3/4" in width.
  - 4. Drilling of rotors is permitted.
- c) Adjustable brake bias and wheel shut offs allowed.
- d) Competition-type master cylinders allowed.

#### **18. SAFETY EQUIPMENT**

- a) Fire bottle system HIGHLY recommended.
- b) Driver uniform will consist of:
  - 1. Fire suit (single layer minimum, one or two-piece).
  - 2. Fireproof gloves.
  - 3. Fireproof shoes.
  - 4. Approved helmet with Snell 95 or higher. Snell 2000 highly recommended.
  - 5. Full-face helmet.
  - 6. Head and neck restraint systems HIGHLY recommended.
- c) Car must be equipped with 5-point safety harness, no more than 3 years old, with minimum of 3-inch belts.
- d) Aluminum, high-backed racing seat mandatory.
- e) Window nets, arm restraints and head & neck restraints HIGHLY recommended.
- f) Medium or high-density roll bar padding must be used.

#### **19. RADIO COMMUNICATIONS**

- (a) NO two-way radios. One-way communication from officials only is allowed.

- (b) Transponders are mandatory and must be mounted on the inner right side chassis frame in a range of 9" to 12" behind the rear axle centre line at a height of 12" to 18" from the ground. Transponder must be mounted vertically with no obstruction between the transponder and the ground. It is recommended that a shield be installed in front of the transponder for protection from debris. Shield cannot be made from metal or carbon fibre.

**20.NO mirrors allowed.**

**21.SPECIFIC REQUIREMENTS FOR CAN AM MOTORSPORTS PARK**

Chief Tech Inspector: Lloyd Larmon llarmon@frontiernet.net

315-767-7819

The current STARS / NYDCA rules will be implemented with the following exceptions:

All drivers must be a minimum of 13 years of age. All drivers under 16 years of age must have a signed waiver by parent or guardian.

Section 2J BODY: 9. A rear spoiler may be used on a trial basis and will be a maximum of 4 inches attached to the rear decklid only.

Section 15. SUSPENSION: Aluminum spindles will be permitted on a trial basis. If any problems are encountered, this will be reconsidered.

Section 10 WHEELS & TIRES: Hoosier Racing Tires plated RC1, RC3, & RC4 will also be allowed in addition to the TUSA tire(s).

Section 11 ENGINE SPECS: Aftermarket airboxes will be permitted. No ram air.

Chapman's Can Am Motorsports Park NOTE: There are engines becoming available with the ability of enhancing driver control of power delivery programmed into the system which could very well be a sophisticated meaning of traction control. This being said-any engine providing this system could be deemed illegal. This will be dealt with on an individual basis at the present time at Chapman's Can Am Motorsports Park. In any situation any controls relating to this system within drivers reach when buckled in will not be allowed. This matter of the driver control of power delivery system will be addressed as required, any and all rule(s), statements, or any other regarding this, is subject to change with no advance notice.

If an issue arises that is not addressed within these rules the Chief Tech Inspector will render the deciding factor.

At any time a discrepancy arises regarding any part/component or entire car being allowed will be the absolute decision of track officials.

Any changes from the car's original manufactures specs must be approved by the Chief Tech Inspector.

In the event that any car and or driver has been competing with a rule infraction in the past, regardless of the infraction or the length of time will have no involvement with the tech decision if found not in compliance to these rules concerning any tech inspection or protest situation.

The above statements apply to all tech inspections and protest situations.  
Any car that becomes overly competitive, action will be taken to determine the reason. Tech Inspectors decision is final.

**SAFETY NOTE:** All rules regarding safety are with the intentions to assist in the defense of injury and in no way offer any assurance of absolute protection. It is the responsibility of driver, crew member(s), car owner(s) and or anyone in the presents of hazardous conditions to maintain their personal safety.

**SAFETY EQUIPMENT:** Rules apply at all times. One way Radio required at all times when on track, Frequency 154.515. Snell-rated SA2000 or newer foil faced helmet designed specifically for auto racing required. Roll bar padding required in driver compartment (Fire retardant recommended). SFI-approved foil fire suit, fire retardant neck brace, gloves and shoes required. Recommended: Fire retardant head sock, underwear, fire bottle system. Car must be equipped with 5-point safety harness, no more than 3 years old, with minimum of 3-inch belts. Some form of head/neck restraint mandatory. Window net & arm restraints recommended.

**CAR SAFETY REQUIREMENT:**

The following are mandatory. Maximum 3 years old 5-point safety harness minimum of 3-inch belts, window net, aluminum high-backed racing seat (foil containment seat recommended).

**Tech INSPECTION Policies and Procedures:** An inspection can be demanded anytime once entered pit gate and or after any race event, any position including did not finish. DRIVER only can refuse a tech inspection and will forfeit the affected event money and points as well as all points accumulated, if any. (lose of accumulated points applies only to refusing a tech inspection-does not apply to refusing a protest). Not being the driver's regular car has no relationship to the outcome regarding any tech inspection, driver or tech official's decision.

**Tech AREA Policies and Procedures:** Questions/input will be addressed as possible by the officials. Verbal or physical action directed toward officials or other racers will not be tolerated. Driver along with anyone else that has been allowed in tech area must remain with their designated car unless otherwise instructed by official(s).

**TECH INSPECTION situation** - Only driver/car will be allowed in tech area, driver must remain in car until otherwise instructed by official(s)-a designated number of crew members per car will be allowed by tech officials IF and WHEN needed.

**PROTEST:** Protesting will very limited. Visual Protest: \$75.00 Major Protest: \$200.00 Chief Tech Inspector will make the final decision with what can be protested. No protest of the engine, transmission or their immediate components allowed. Chief Tech Inspector has the authority to disallow any protest he feels inappropriate or has not properly followed protest procedures and his decisions) will be final concerning any issues. Driver only can refuse to be protested and will forfeit the money and points for the affected event only. There are two types of protest, visual or major. Protest must be LEGIBLY hand written IN DETAIL, with EXACT amount CASH only US funds, submitted by driver or one crew member to Tech Inspector within 10 minutes of end of feature. Visual or major protest.

PROTEST situation - Only driver/car being protested will be allowed in tech area-a designated number of crew members will be allowed by tech officials IF and WHEN needed.

FOUND LEGAL: Car being protested receives protest money. FOUND ILLEGAL: Protester

is refunded protest money.

Any protested item deemed illegal by Chief Tech Inspector will render car and driver disqualified from the affected event. Not being the driver's regular car has no relationship to the outcome regarding any protest.